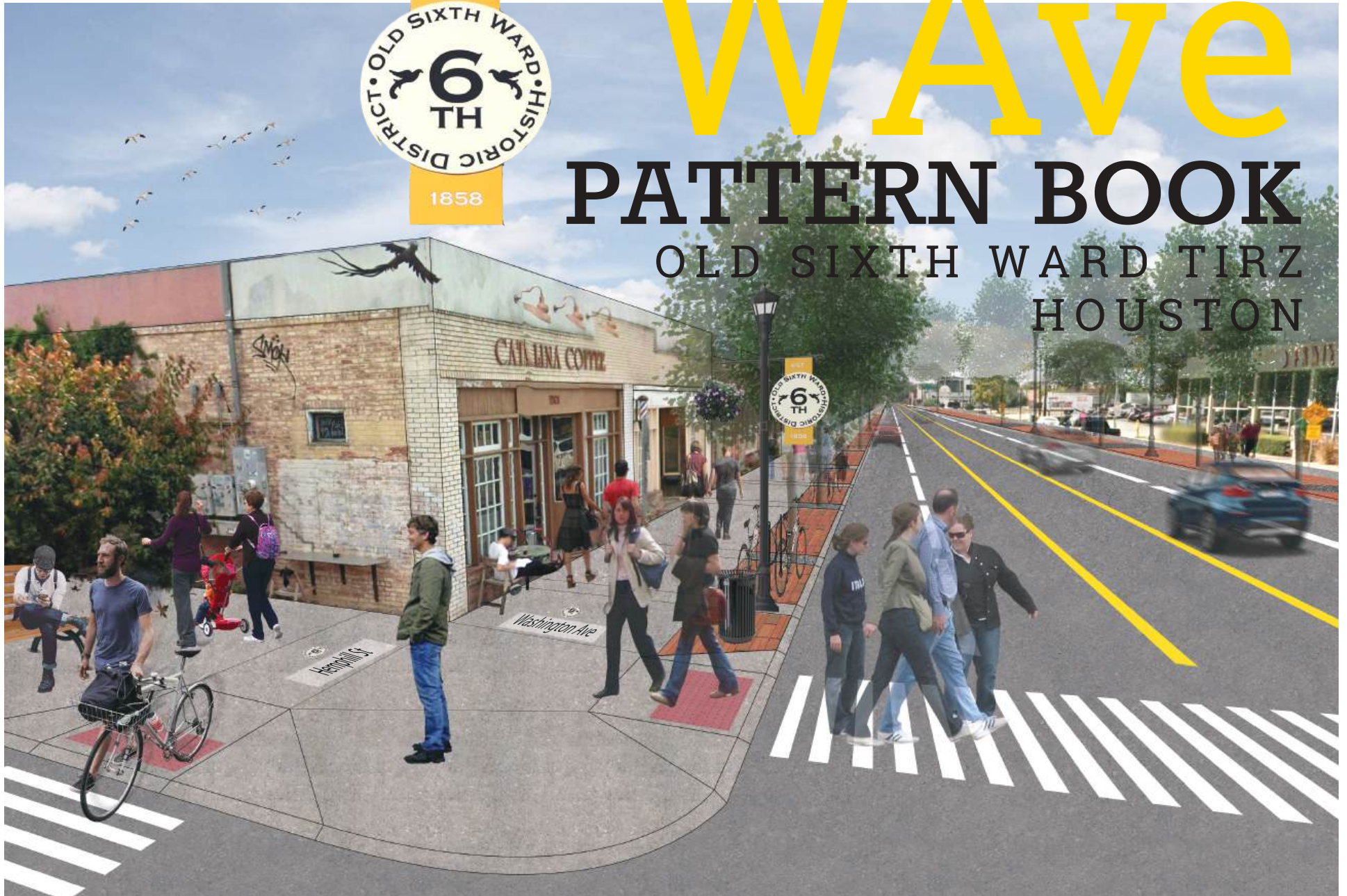




Wave

PATTERN BOOK

OLD SIXTH WARD TIRZ
HOUSTON





This report prepared for the Old Sixth Ward
TIRZ by Asakura Robinson, February 2017

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01 INTRODUCTION

The Washington Avenue Pattern Book is intended to provide direction for the development and public realm aesthetics of Washington Avenue and the adjacent major streets, including Sawyer, Edwards and Houston Streets, within the Old Sixth Ward Tax Increment Reinvestment Zone.

Building off of the Washington Avenue Livable Centers Plan and the Sawyer Street Complete Streets Concept Plan, as well as the public realm improvements completed by the Tax Increment District within the historic core of the neighborhood, the pattern book provides guidelines for redevelopment of vacant parcels which seek to receive TIRZ reimbursement and sets a standard for public realm improvements with a goal of providing a uniformly superior experience for residents, business and building owners, employees, shoppers, diners and other visitors to one of Houston's most unique historic assets.





02 EXISTING CONDITIONS

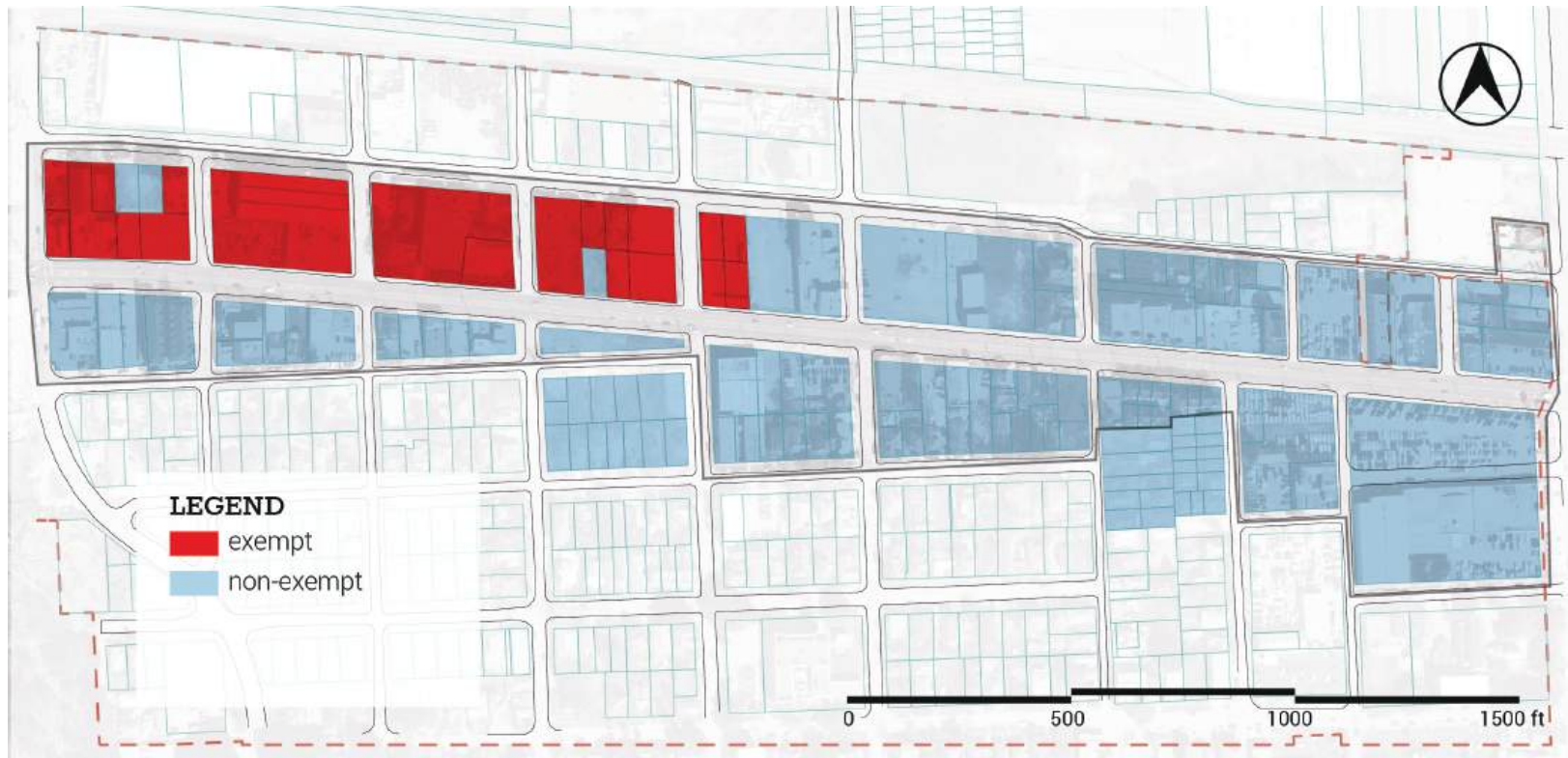
Over the previous decades, the Washington Avenue Corridor has undergone significant changes. In the period since the completion of the Washington Avenue Livable Centers Plan in 2012, at which time much of the stretch of Washington between Houston and Sawyer Streets was vacant, a number of new restaurants, bars, and retail have opened their doors within previously unused buildings, and significant new development is planned. Lower Washington is emerging as a key retail and restaurant corridor with a unique historic building stock, however, the public realm remains intermittent and sparse in its ability to provide an experience that encourages livability and walkability.

PARKING LOTS AND ACTIVE FRONTAGE



A large proportion of the Washington Avenue Corridor is dedicated to surface parking. However, as the area has become a nightlife and arts destination, new businesses are opening and creating active uses along the entire length of the corridor from Houston Ave. to Sawyer St.

EXEMPT PROPERTIES



North of Washington Avenue, the western portion of the study area is held by several non-profit and religious organizations. This is significant because the TIRZ will not realize any revenue from these buildings unless they change ownership. Because the non-profits do not have to pay property tax, they will feel less pressure to redevelop or sell these parcels as land values and taxes rise.

LAND VALUE PER SQUARE FOOT



Most properties are valued between \$35 to \$42 per square foot, making townhouses and smaller multifamily developments up to six stories the most efficient use of the land, depending on parcel size. One major outlier is the triangular property, valued at below \$10 per square foot. This property will be extremely difficult to develop unless it can be developed in conjunction with the block to the south. The property may be attractive as a pocket park.

IMPROVEMENT TO LAND VALUE RATIO



Most properties in the study area have a very low improvement to land value ratio, indicating that they are ripe for redevelopment or significant reinvestment.

SIGNIFICANT OWNERSHIP



Much of the property north of Washington Avenue has already been consolidated by larger property owners. Some of these parcels are held with the intention of redevelopment and will likely be redeveloped in the next two years.

With the exception of the 815 Houston Ave LLC / Auto Dealership properties, most of the frontage south of Washington Avenue consists of smaller individually owned properties. Because of the smaller holdings and irregular block shapes, this side of the Avenue will likely see smaller development projects or adaptive reuse.



03 GUIDELINES FOR NEW DEVELOPMENT

The design of new buildings is critical to preserving and strengthening the character of a historic district. While architectural styles, details, and materials are often emphasized, the scale, rhythm, and use of a building is equally important and often more difficult to reconcile with buildings designed for a different context in a different time.

In the case of Washington Avenue and the Old Sixth Ward, the urban fabric was defined by small single-family houses, light industrial buildings for logging and other light industry, with smaller one and two story commercial buildings along Washington Avenue. There are some opportunities for adaptive reuse, but most development will be new construction on the numerous surface parking and vacant lots along the stretch of Washington Avenue between Sawyer Street and Washington Avenue.

As the area evolves, most new buildings will be three to four story mixed-use multi-family and townhouses. Thus the challenge of strengthening the historic character of the Old Sixth Ward will be to design these new modern building typologies in a way that fits in and complements the remaining historic fabric.

The following guidelines should be applied, where possible, to all new development and redevelopment on Washington Avenue.

URBAN DESIGN

The diagonal orientation of Washington Avenue results in a slightly irregular grid, based on a block size of 320' by 220', with each block originally split into 12 parcels 50' wide and 100' deep. Throughout the years, some parcels have been combined, but most do not exceed 1/3 of a block.

Façade Rhythm and Orientation

New development should respect the historic 50' and 100' lot widths. Larger developments may be broken into multiple buildings or clearly articulated façade variations and multiple entrances along a block for residences and retail tenants.

- Entrances should be no more than 100' apart
- Façades should be differentiated horizontally every 50' or 100.'
- New buildings should be oriented toward the street with service and parking facilities at the rear, side, or within internal alleyways
- Smaller retail spaces 50' to 100' wide are encouraged.
- All primary building entrances should comply with Texas Accessibility Standards

Building Height

Most historic buildings are less than two stories, but most new development will likely be 3 to 5 stories tall. New development should complement the existing buildings on a block by creating a clearly articulated one or two story base.

- A one- or two-story base should be clearly articulated through architectural details or a single stepback
- Above the fourth floor, buildings should be stepped back by at least 20'. Buildings above six stories are discouraged.
- Towers are strongly discouraged.
- Variations in height along the width of a building should follow the original 50' and 100' wide parcels.

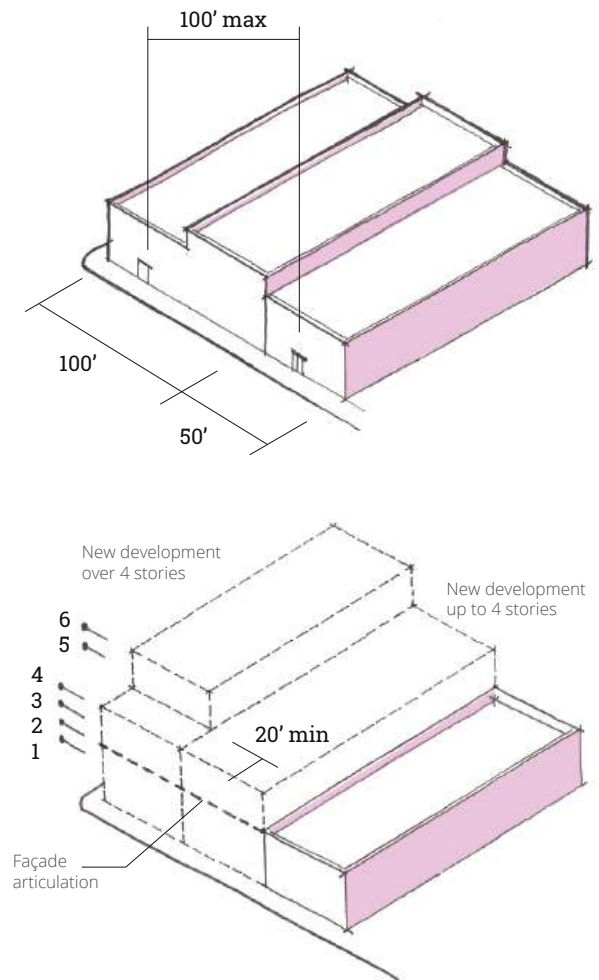


fig. 1
Typical Façade Rhythm
& Building Height

Setbacks and Curbs

The vast majority of historic structures along Washington Avenue are set directly at the property line. Infill projects should respect this street wall or make allowances for public spaces between the building and the street. These spaces should either be an extension of the public realm and comply with the design standards for the public realm, or be clearly delineated as patios, gardens, etc. with landscaping, planters or low fencing.

- New buildings should respect existing streetwall where appropriate.
- New buildings may be set back up to 20' to create active, well-designed spaces for retail uses.
- Existing curb cuts should be reduced or eliminated wherever feasible.

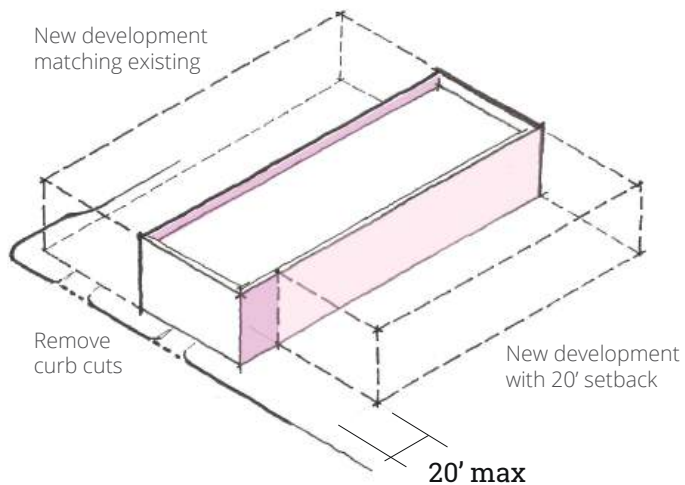


fig. 2
Typical Setbacks

Parking

The historic buildings were built before automobiles and parking were major influences on urban design. Integrating space for parking is a key challenge of reinterpreting historic building styles for a modern context.

- Parking should not be located between buildings and the street.
- When financially feasible, underground parking and wrapped structured parking is encouraged. Otherwise, parallel parking should be located along side streets or internally.
- Shared parking arrangements are encouraged for smaller developments.

ARCHITECTURAL STYLES

The predominant style for commercial buildings is masonry block, typically brick. While most of the buildings along Washington Avenue are two stories or less, this common historic building typology commonly had up to four stories. Most existing buildings have a horizontal expression with a ground floor broken into smaller retail units and the upper floors retaining a more continuous, monolithic surface with punched windows.

Materials and Color

Most of the historic structures have yellow or light red brick facades with a small amount of architectural ornamentation expressed through brick embellishments or terracotta inlays at the parapet, around windows, and above the ground floor. Many buildings feature wooden storefronts slightly recessed into the building.

- Buildings should be primarily constructed out of light-colored unglazed brick, or similar masonry.
- Painted wood or other materials inspired by the traditional retail facades are encouraged on the ground floors.
- Cast iron or other dark metals that reflect the industrial nature of the area may also be used as accents.

Glazing and Fenestration

Wood-framed retail storefronts give the historic buildings along this corridor a distinct style by creating a rhythm that follows the 50' wide parcel structure, creating connections to the street, reinforcing the pedestrian scale, and adding accent colors to the standard brick façade. Above the base, small windows punched out of the façade are often in groups of three to subtly reinforce the 50' property lines.

- Grouping retail facades into 50' units inspired by the wooden storefronts is encouraged.
- Simple, vertical windows are in groupings that reflect the original 50' property lines are encouraged for the upper stories.
- Larger stretches of glazed walls should be avoided.
- A glazed transom – the upper section of a ground floor retail – is encouraged



Details and Ornamentation

The historic buildings are simple structures with minimal detailing other than framed retail spaces, a simple brick cornice, and simple brick or terracotta details to emphasize the transom and distinguish the ground floor from the private upper floors.

- Complex non-rectilinear architectural forms should be avoided.
- The primary function of details and ornamentation should be to distinguish the upper floors from the ground floor retail.
- Brick patterning and terracotta inlays area encouraged. Dark metal that reflects the areas industrial history is also appropriate.
- Avoid excessive ornamentation of building facades in keeping with simple detailing of existing buildings; simple ornamentation is recommended to articulate first and second stories of new buildings.

✓ STORY DIFFERENTIATION



✓ AWNINGS



✓ PAINTED SIGN



Signage and Awnings

- Awnings are strongly encouraged for all ground floor retail and entrances. Metal chain and escutcheon supported canopies are most appropriate.
- Signage should be minimal and inspired by historic commercial designs. Transom signs and small to medium blade signs are encouraged. Small signed hanging from awnings and signs or murals painted directly onto the brickwork are also appropriate.
- Plastic signs should be avoided.

Landscape Features

Because most buildings will be located at the property-line, there will be minimal space for planting.

- Where possible, green infrastructure, including rain gardens, is strongly recommended.
- Plantings should be kept less than 18" in height and maintained such that they do not interfere with pedestrian movement at entrances and right-of-way.



04 PUBLIC REALM STANDARDS

The existing and future development along Washington Avenue is tied together by the streetscape. Public realm improvements are important to produce a unified visual language for the corridor and promote furnishings and street treatments that encourage pedestrian comfort and safety and promote vibrant social and business life along the street.

The following standards should be applied, where possible, to all new development and redevelopment on Washington Avenue:

The following public realm standards should serve as a guide for the right-of-way improvements that will be expected with incremental development projects. Street trees and lighting will promote pedestrian comfort, providing both a conceptual buffer between the sidewalk and road and shade and light for user comfort and safety throughout the day and night. Curb improvements will help the Avenue meet accessibility best practices as well as reduce conflicts with turning vehicles and unsafe turning vehicle speed. Site furnishings, including bicycle racks, trash and recycling bins, and benches, will support daily life and activities along the street.

Right of way and Sidewalks

The desired right-of-way width is 80'.

- Corner and curb radii shall maintain a 10" radius at all corners, including driveways and alleys (requires approval by Transportation and Fire Department); otherwise maximum radius of 15'.
 - The standard sidewalk width shall be 10'-0"* (to face of curb), including a 6' clear Pedestrian Zone (from property line outward) and 4' Planting zone (remaining to, and including, 6" curb) for trees, utility access, and street furnishings.
 - Pedestrian Zone paving shall be concrete, broom finish, per the City's standard detail.
 - The City's standard detail shall be used for all curb and gutter construction.
 - CU-Structural Soil is recommended for entire length of planting area outside of tree wells to support previous paving and allow for continuous stormwater flow; this will improve the health of trees and reduce heaving of pavement.
 - Street names should be engraved in sidewalk at intersections, with accompanying Old Sixth Ward logo.
- * At certain locations, 10' width sidewalks may not be possible due to existing conditions. The TIRZ will evaluate these situations on a case-by-case basis.

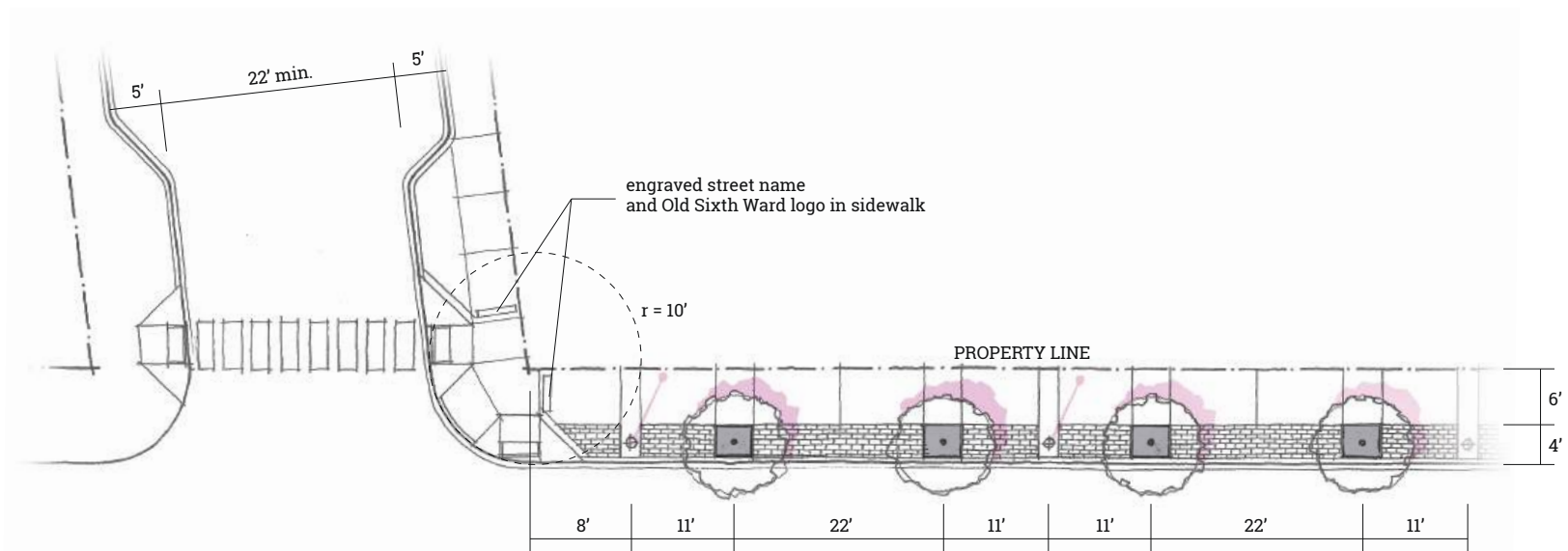


fig. 3
Typical Street Configuration & Placement

Street Lights

- Street lights shall be placed 44'-0" o.c. along all blocks, beginning with a corner light pole placed 8' from interior edge of corner ramp.
- Streetlight poles adjacent to street intersections shall be 2'-6" inward from property corner (coincides with interior edge of corner ramp); then commence the 44'-0" pole to pole spacing;
- Provide at least 11'-0" distance between light poles and centerline of trees;

Recommended model: Lighting Associates Geneva Series 10' Black Textured Pole, 87-10-FP4-X-FP4-BKT with Architectural Area Lighting Promenade Series LED, Black, PRMS-T5-48LED-4K-700-BL

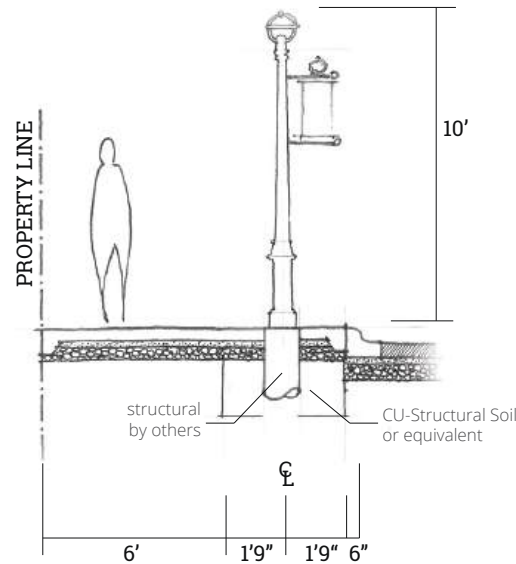


fig. 4
Typical Street Light
Installation

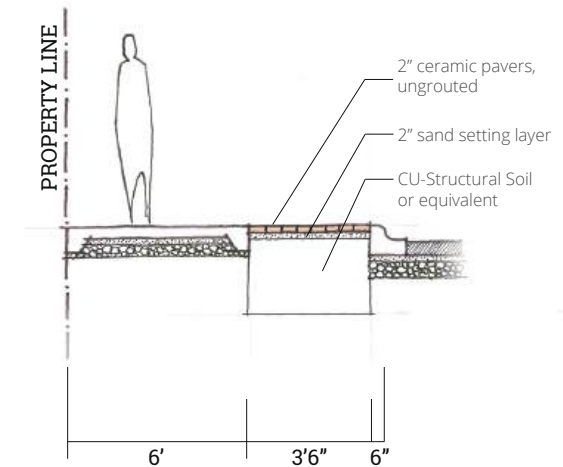


fig. 5
Typical Planting Zone Paver
Installation

Pavers

- Planting zone paving shall be clay brick pavers on 2" sand, excluding tree wells and 2' wide concrete band at street light bases
- Pavers should not be grouted to allow for percolation of stormwater into engineered soil mixtures

Street Trees

Trees shall be a minimum of three-inch (3") caliper (measured 12" above the root ball) at installation.

- Smaller ornamental trees shall be used in all areas beneath existing or proposed utility lines; where no lines exist, larger shade species shall be used
- Trees shall be spaced 22' o.c. beginning 11' from corner light pole and on centerline of 4' wide planting zone (2' b.o.c.)
- Tree wells shall, at a minimum, be 4' long by 3'-6" wide by 3' deep.
- Typical rail ballast shall cover entirety of tree wells where groundcover is not included
- Where existing sidewalk is less than 10' in width, no street trees are required.

RECOMMENDED SPECIES

Under Utility Lines	Not Under Utility Lines
Black Cherry <i>Prunus serotina</i>	Drummond Red Maple <i>Acer rubrum var. drummondii</i>
East Palatka Holly <i>Ilex x attenuata 'East Palatka'</i>	Mexican Sycamore <i>Platanus mexicana</i>
Texas Persimmon <i>Diospyros texana</i>	Monterrey Oak <i>Quercus polymorpha</i>
	White Ash <i>Fraxinus americana</i>

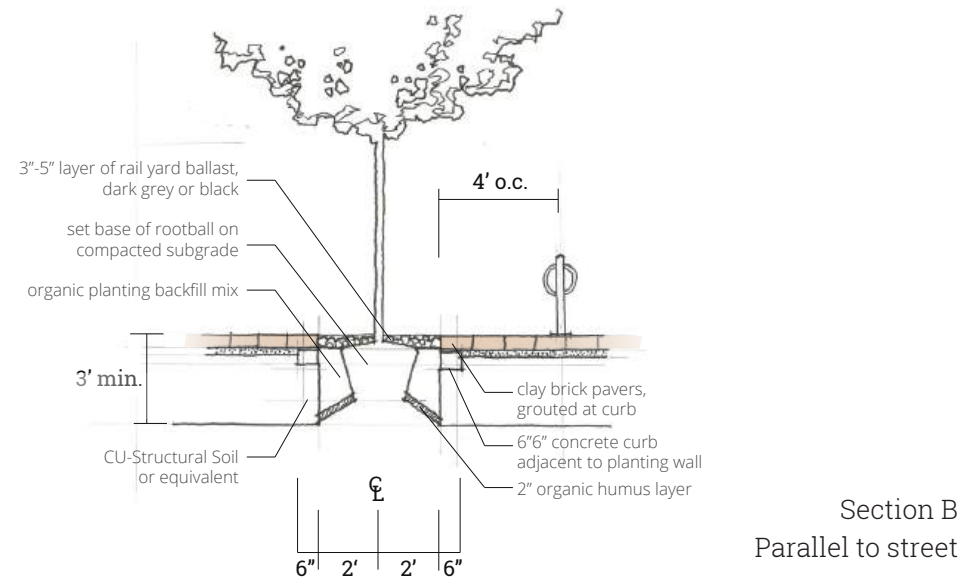
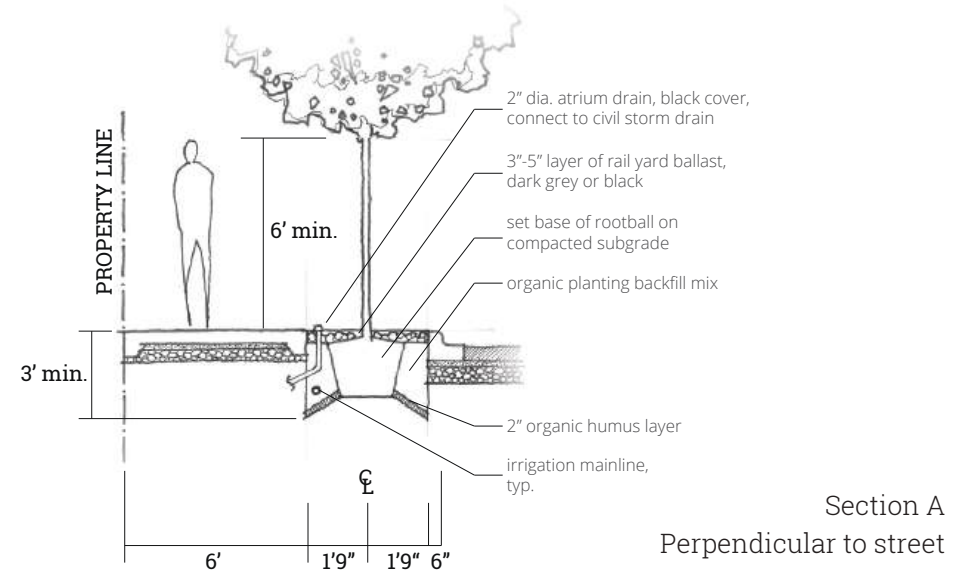


fig. 6
Typical Street Tree Installation

Benches

4 per block face

- 6' or 4' benches installed parallel to street between trees, facing toward building.
- Locate 6' benches close to existing bus stops, parallel to and facing the street where possible.
- 4' benches at intersection build-outs (see figure 8).

Recommended models: Victor Stanley Classic Collection C-138, 48" Black powdercoat finish with hardwood seating, surface mounted; Landscape Forms Plainwell Bench, 72", Black powdercoat finish, hardwood slats, no center arm



Bike racks

8 per block face

- Tubular ring racks shall be installed parallel to street at centerline of planting zone.
- Racks should be installed in pairs near building entrances or adjacent to corner ramps.
- Allow for 3' o.c. between racks and 4' clearance to other furnishings.

Recommended model: Victor Stanley Cycle Sentry Collection BRBS-103, Black powdercoat finish, surface mounted

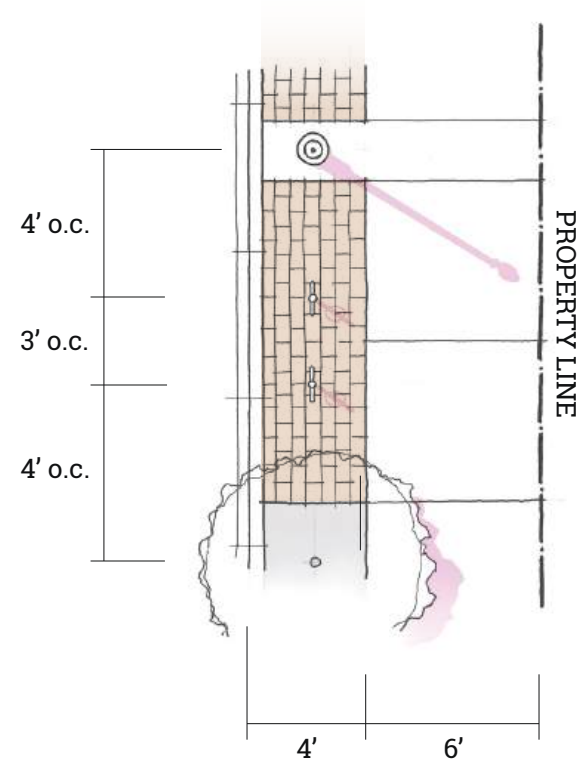


fig. 7
Typical Bike Rack Placement

Waste and Recycling Bins

2 per block face (1 waste, 1 recycling)

- Bins shall be located near building entrances or benches if mid-block site or adjacent to corner ramps if site is located at a corner, centered between trees and/or furnishings.

Recommended models: Landscape Forms Plainwell Litter Receptacle, black powdercoat finish, top open lid; Victor Stanley Ironsites Collection S-42, black powdercoat finish, tapered formed or recycle lid



S U M M A R Y

Street Elements	Spacing / Location *
Trees	22' o.c.
Lights	44' o.c.
Utility Poles	88' o.c.
Benches	4 per block
Waste Bins	2 per block
Bike Racks	8 per block

* On blocks less than 200' in length, total bike racks and benches may be reduced by 1/2 and recycling bin maybe omitted.

Utility Poles

Existing utilities poles are placed primarily along the southern sidewalk of Washington Avenue. New development is unlikely to significantly alter the placement of these existing poles.

- When applicable, place new utility poles at 88' o.c.
- Provide at least 11'-0" distance between utility poles and centerline of trees;
- Smaller ornamental trees should be used beneath existing utility lines (see recommended species chart, p.21)

Fencing

Fencing along pedestrian areas is discouraged to allow for visible and physical interaction

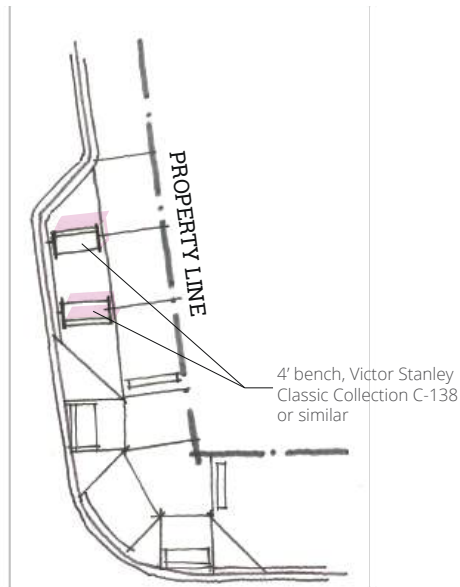
between building fronts and pedestrian traffic and to encourage an atmosphere of openness.

- Fencing shall be visibly permeable and comply with Texas Accessibility Standards
- No front fencing shall exceed 48" in height.

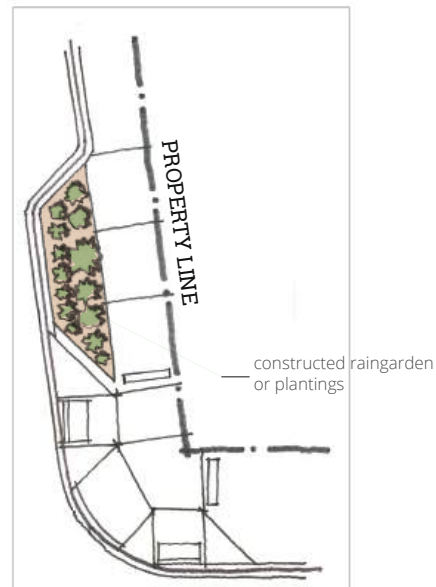
Bulb Outs

Traffic entering and exiting neighborhood streets perpendicular to Washington Avenue may be calmed by decreasing the width of the intersection using bulb out curbs. These bulb outs decrease crossing distance for pedestrians, increase pedestrian visibility, and reduce vehicle speeds at crosswalks

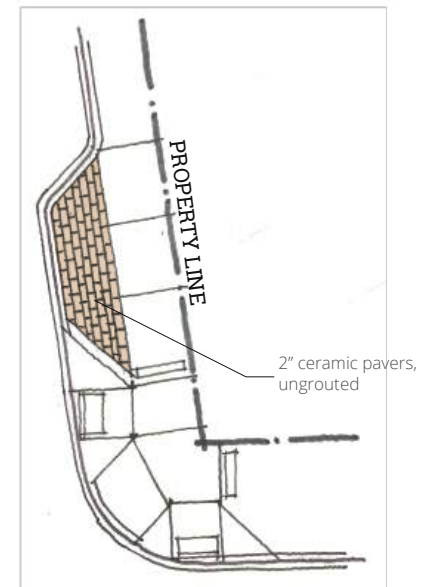
- Bulb outs are not required at intersections but are highly encouraged.
- Bulb outs should be a minimum of 5' in width from existing curb, except where street would be made more than 22' in width.
- Bulb outs should not be more than 9' in width.
- Bulb out length may vary but must be a minimum of 20' except where prohibited by existing obstacles or driveways.
- Three options (shown) for bulb out treatments are recommended:



(2) 4' BENCHES OPTION



RAINGARDEN/PLANTING OPTION



PAVERS OPTION

fig. 8
Intersection Bulb Out Treatment Options



APPENDIX: CASE STUDIES

The development of the standards in chapters 3 and 4 were dependent on an examination of existing historic and new buildings both within the TIRZ and in other areas of the Washington Avenue Corridor. The findings of those examinations are provided below.

1702 WASHINGTON AVENUE: FIRE STATION 06

HCAD Data

Owner	1702 Washington Ave LTD
Legal Description	LT 3 & 4 Heiss
State Class Code	F1 -- Real, Commercial
Building Class	E
Land Area	15,000 sqft
Total Living Area	9,256 sqft
Land Value (Market)	600,000
Improvement Value (Market)	672,711
Construction Type	Fireproof Concrete (Building 1) Wood / Steel Joist (Building 2)
Year Built	1903 Building 1 2011 Building 2
Type	Office Bldgs. Low Rise

General Information

Building Height	2 Story 28 ft
Materials	Building 1: Red Brick Exterior Flat Roof with Brick Parapet Roof Tile detail on Facade Building 2: Brick and Concrete
Property Line Setback	-4 ft (6ft curb)
Parking	2 No. Accessible Spaces 15 No. Standard Spaces
Access	Pedestrian Entry via parking lot Vehicle access off Washington Ave

URBAN ANALYSIS

- 😊 Landscaping provides a defensible, aesthetic edge.
- 😊 Buildings are at a comfortable pedestrian scale.
- 😊 Small curb cut to enter parking lot
- 😞 Sidewalk is not sufficient for increased pedestrian activity or street furnishings.
- 😞 Closing entrance via Building 2 off Washington Ave is a missed opportunity to increase neighborhood 'friendliness' and safety, particularly since a door opening already exists.



View from Washington Avenue



1814 WASHINGTON AVENUE:

'BUTCHERS AND RESTAURANT'

HCAD Data

Owner	Dittman on Washington LTD
Legal Description	LTS 5 6 7 10 11 & 12 & TRS 4A 8A 9A & 13A BLK 445 BAKER W R NSBB
State Class Code	F1 -- Real, Commercial
Building Class	E
Land Area	37,518 sqft
Total Living Area	7,250 sqft
Land Value (Market)	1,500,720
Improvement Value (Market)	59,834
Construction Type	Wood / Steel Joist
Year Built	1940 (Remodeled 2015)
Type	Restaurant

General Information

Building Height	2 Story 28 feet
Materials	Red Brick Exterior Shop front windows Flat roof
Property Line Setback	-4 ft (6ft curb)
Parking	3 No. Accessible Spaces 45 No. Standard Spaces
Access	Pedestrian Entry off Washington Ave Vehicle access off Washington Ave Service access off Center

Urban Analysis

- 😊 Over 50% glazing on street level.
- 😊 Direct access into building off Washington Ave.
- 😊 Aesthetically pleasing signage.
- 😊 Buildings are at a comfortable pedestrian scale.
- 😞 High % of impervious cover with asphalt parking lots to each side, increasing ambient temperature.
- 😞 Large curb cuts (2 way)
- 😞 Sidewalk is not sufficient for increased pedestrian activity or street furnishings.



View from Washington Avenue



4600 WASHINGTON AVE LTD

HCAD Data

Owner	Washington 4600 LTD
Legal Description	RES A BLK 1 WASHINGTON CENTRE AT PARKER R/P
Tenants	The Counter burger chain Commonwealth Rfb Port Neches Les Givral's Washington
State Class Code	F1 -- Real, Commercial
Building Class	A+
Land Area	62,957 sqft
Total Living Area	102,620 sqft
Land Value (Market)	3,147,863
Improvement Value (Market)	11,057,778
Construction Type	Fire Resistant Steel (Building 1) Fireproof concrete (Building 2)
Year Built	2009
Type	Office Buildings Low Rise (Building 1) Parking Garage (Building 2)

General Information

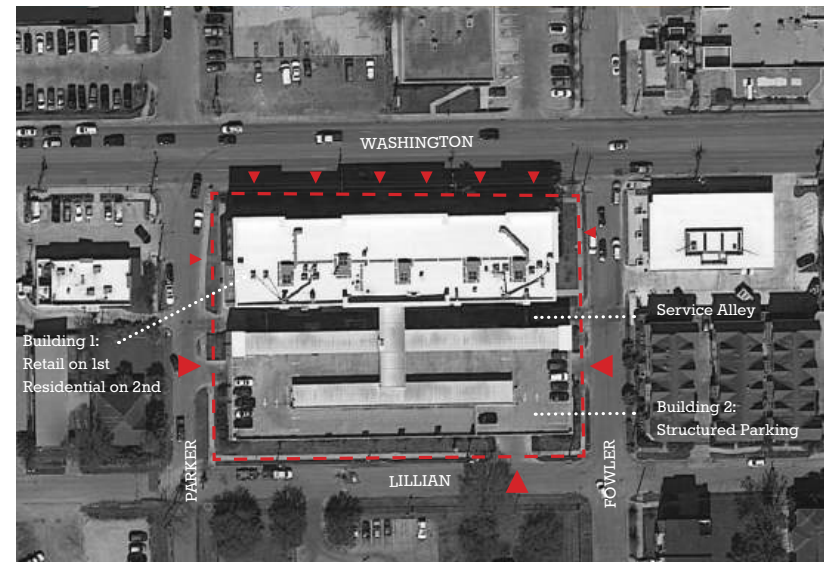
Building Height	3 Story (Building 1) 2 Story (Building 2)
Materials	Red Brick and Stucco Exterior Shop front windows Flat roof
Property Line Setback	10ft
Parking	Structured parking
Access	Pedestrian Entry off Washington Ave Vehicle access to Structured Parking via Lillian, Parker & Fowler Service access from Parker & Fowler

Urban Analysis

- 😊 Retail and storefronts to street make a pedestrian friendly experience.
- 😊 Structured parking at the rear with vegetated edge improves quality of Washington Ave. but also considers residential neighbors.
- 😊 Building scale is broken down by change in elevation treatment. Stucco and brick alternate.
- 😊 Canopies and landscape provide shade.



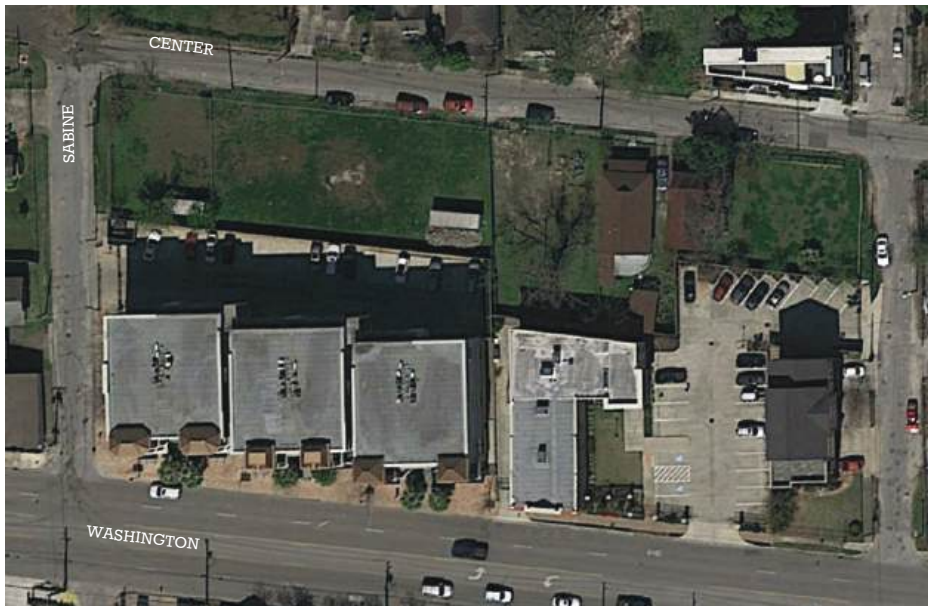
View from Washington Avenue



1708-22 WASHINGTON AVENUE: WASHINGTON PLACE LOFTS CONDOS



View from Washington Ave



Site Plan

HCAD Data

Owner	Various (25 Residential Units Total)
Legal Description	WASHINGTON PLACE LOFTS CONDO
Tenants	Showcase Properties Eye candy Lashes Allstate Insurance Galerie barre Relion Investments LLC
State Class Code	A1 -- Condo - High Rise
Market Value	7,112,387
Construction Type	Fire Resistant Steel (Building 1) Fireproof concrete (Building 2)
Year Built	2001
Type	Office Buildings Low Rise (Building 1) Condo

General Information

Building Height	3 Story (Building 1) 2 Story (Building 2)
Materials	Red Brick and Stucco Exterior Ceramic roof tile
Property Line Setback	5-10ft
Parking	1 No. Accessible Spaces 18 No. Standard Spaces
Access	Pedestrian access off Washington Ave Vehicle access to surface Parking via Sabine



dSakura
robinson